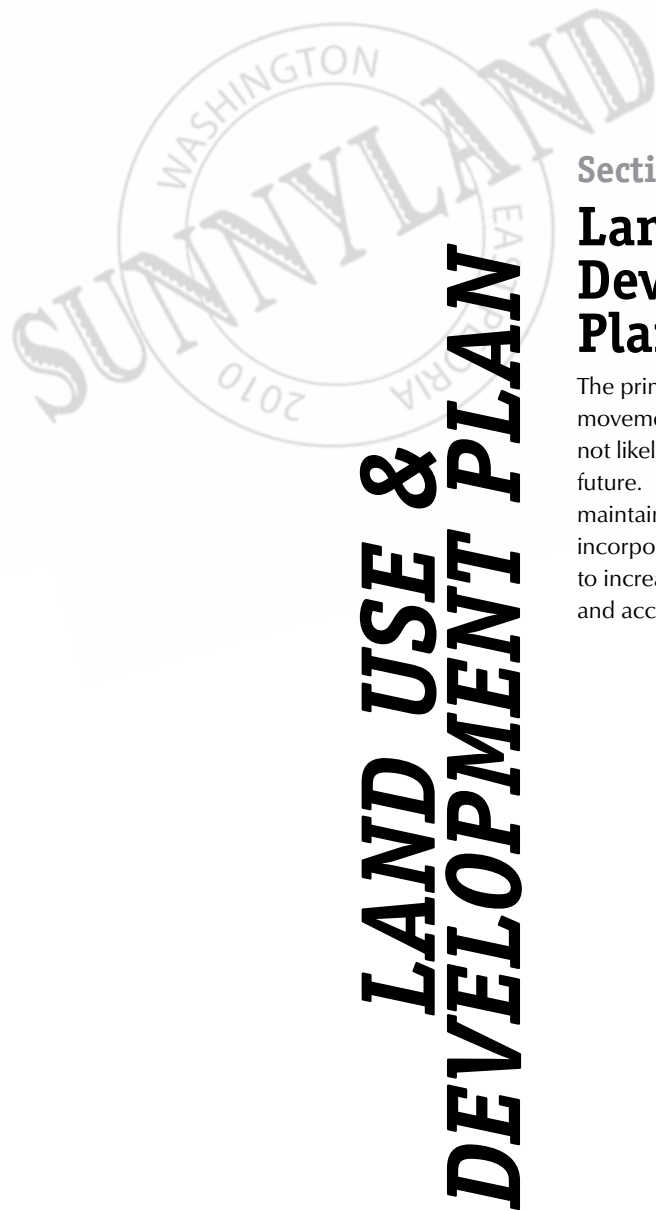




LAND USE/DEVELOPMENT PLAN

section four



Section Four

Land Use and Development Plan

The primary purpose of Route 8 is the movement of goods and people. This is not likely to change in the foreseeable future. It is possible, however, to maintain this necessary function while incorporating redevelopment strategies to increase the area's economic vitality and accommodate pedestrians.

The Land Use and Development Plan (Figure 6) represents the community's desire and vision for the Corridor and serves to guide decision-making in the future. The Plan illustrates which uses and types of development are desired within the Corridor. It suggests changes in land use and possible development scenarios that could be achieved. The Plan seeks to encourage investment in the Corridor through a cooperative relationship with property owners and developers. The Land Use and Development Plan incorporates sound planning practices and reflects market realities and community input.

Future land use and development within the Corridor is influenced by many factors, most notably a parcel's location. The Land Use and Development Plan provides recommended land uses that are sensitive to nearby and adjacent uses, identifies enhancement sites and key opportunity sites.

Residential

Goal

Preserve and protect the established residential neighborhoods from adjacent and nearby commercial/mixed-use development and activity, and provide appropriate new multi-family and single-family attached residential units within the corridor in appropriate locations, as part of larger mixed-use developments.

Objectives

1. Protect established residential neighborhoods from encroachment by incompatible land uses and the adverse impacts of adjacent activities.
2. Ensure that residential areas have adequate buffering and/or screening from adjacent land uses.
3. Promote the economic and overall community importance of the diversity of the Corridor's housing stock to provide high quality housing for all stages of life.
4. Encourage new development and redevelopment that is respectful of the scale and character of surrounding residential uses.

Commercial/Mixed-Use

Goal

Create an attractive commercial/mixed-use environment along Route 8 that attracts businesses offering daily needs for goods and services in addition to destinations with a larger customer draw by accommodating multiple modes of transportation to and through the Corridor.

Objectives

1. Locate and design new commercial/mixed-use developments in a manner that strengthens the Corridor's overall character and sense of place.
2. Attract businesses that provide daily goods and services in addition to developing selected "destination" commercial shopping opportunities, particularly at Route 8 and Summit Drive. Encourage larger development sites through parcel assembly of smaller adjacent properties.
3. Encourage the establishment of family restaurants within the Corridor, as either "stand alone" development or as part of larger commercial/mixed-use developments (restaurants should not be established adjacent to existing single-family residential areas).
4. Encourage new commercial development and redevelopment that is respectful of the scale and character of nearby residential areas.
5. Ensure that all commercial/mixed-use development provides sufficient setbacks, buffers, and screening to adequately protect adjacent residential areas from non-residential activity.
6. Encourage parking areas to be located at the rear of buildings, rather than as prominent development features along Route 8.
7. Minimize curb-cuts along Route 8 by encouraging shared driveways and internal cross access of adjacent parking areas wherever possible and appropriate.
8. Encourage new development to provide pedestrian amenities, plazas, gathering places, and other distinguishable design features.
9. Support a proposed name change for Sunnyland Plaza.
10. Develop land use and economic development goals for the proposed extension of Summit Drive as a catalyst for new growth.

Community Facilities and Open Space

Goal

Provide additional parks and open space as part of new development when possible and appropriate, and explore opportunities for new community facilities within the Corridor.

Objectives

1. Seek opportunities to partner with other agencies to development programmable open space for use by area residents.
2. Provide passive open space areas as part of larger developments.
3. Given the opportunity, seek to establish a new public park in front of the Tall Oaks development.
4. Provide a network of sidewalks and trails connecting open spaces, parks, residential neighborhoods, and shopping and service areas within Sunnyland and beyond.

Figure Six Land Use Recommendations

Land Use

- Single Family Residential
- Multi-family Residential
- Open Space
- General Commercial
- Commercial Service & Office
- Public & Semi-Public
- Mixed-use

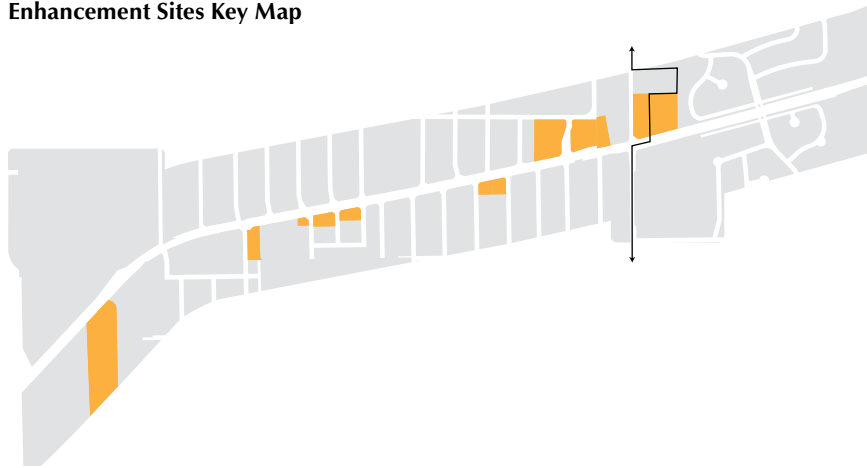
Additional Map Features

- Municipal Boundary
- Study Area Boundary



This figure illustrates land use recommendations over the long term and is based on sound planning principles and market realities.

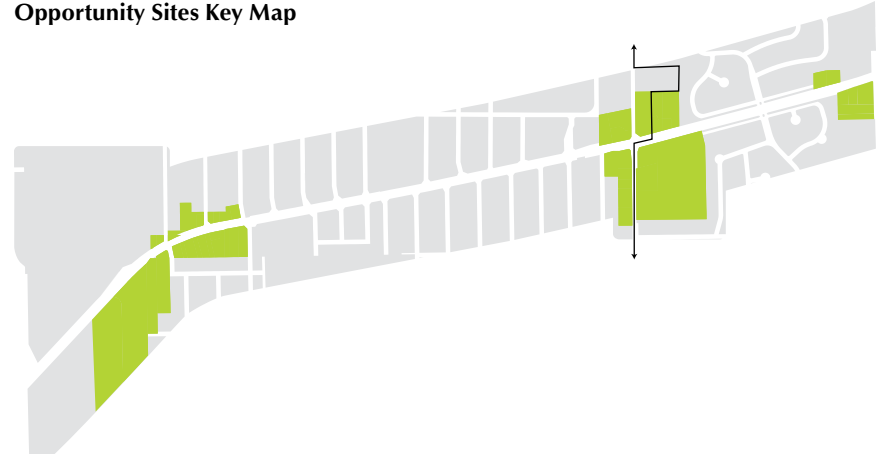
Enhancement Sites Key Map



Enhancement Sites

Enhancement sites are properties, buildings, and areas where aesthetic changes have the ability to improve the look and function of the corridor as a whole. Figure 7 details sites and recommendations for possible enhancements. Additional information is provided in the Beautification Plan section.

Opportunity Sites Key Map





Opportunity Sites

To complement new construction, private investment and proposed enhancement sites, Figure 8 demonstrates opportunity sites where redevelopment efforts should be concentrated.

Through parcel assembly, cooperation with private owners and public-driven incentives, these opportunity sites have the potential to dramatically improve the corridor and act as a catalyst for economic development.

Figure Seven Corridor Enhancement Opportunities

Map Features

-  Municipal Boundary
-  Study Area Boundary

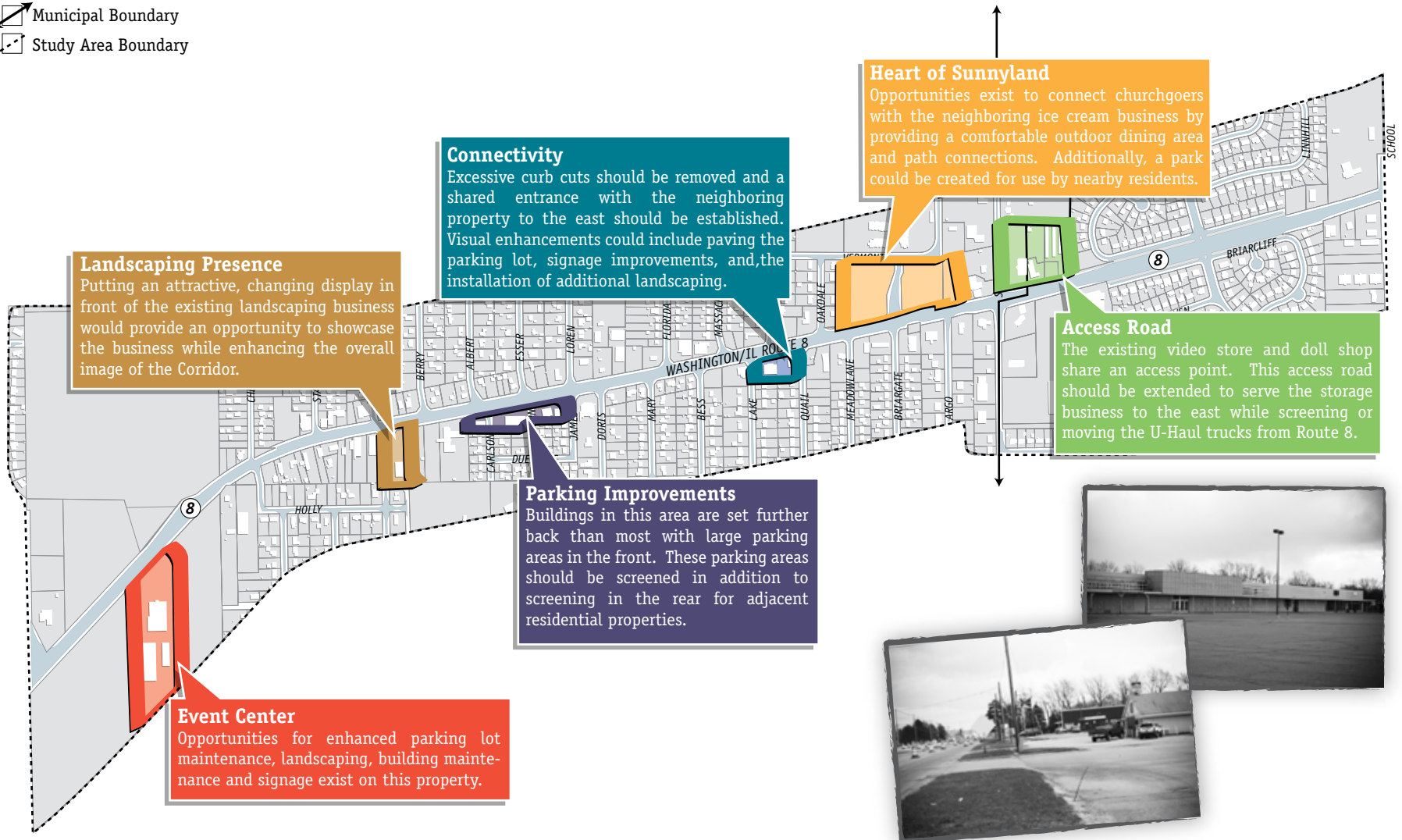




Figure Eight Opportunity Sites

Map Features

-  Municipal Boundary
-  Study Area Boundary

Opportunity sites are those sites with the greatest potential to have a lasting, positive impact on the corridor both in terms of physical appearance/function and economic development. The following redevelopment scenarios are meant to be visionary and do not prescribe a specific recommendation for redevelopment.

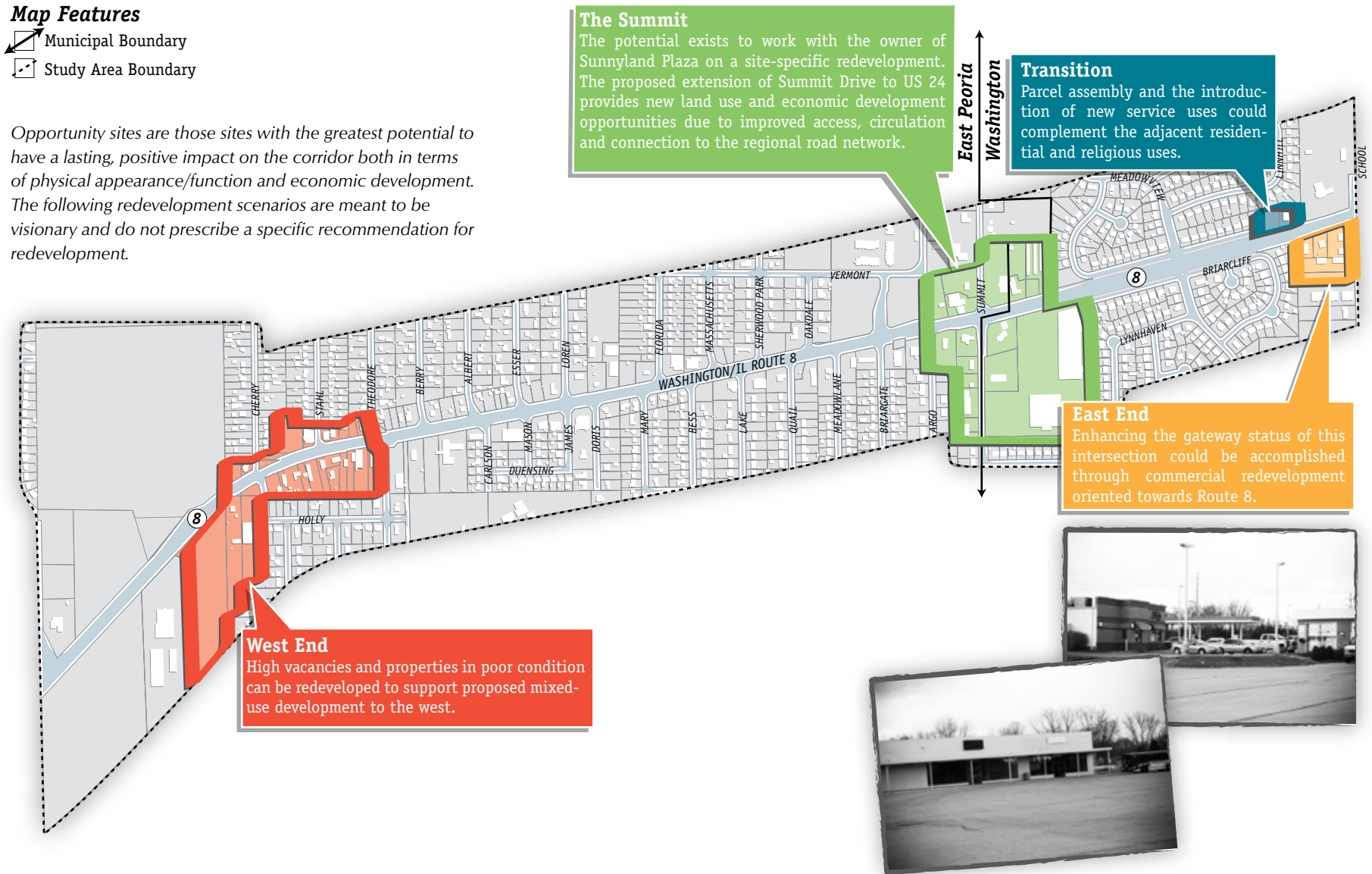


Figure Nine Illustrative Redevelopment Concepts • Existing Conditions

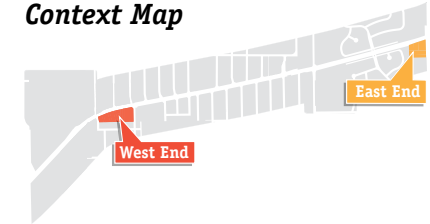
Illustrative development concepts have been created for two sites along the Corridor—one on the West End and one on the East End. For each site, two development concepts were created to demonstrate a vision for what is possible when and if these areas redevelop. These scenarios are provided for illustrative purposes only and are not meant to provide a specific recommendation.

On the following pages, **Concept A** features a multi-building approach while **Concept B** illustrates a single building approach that maximizes site capacity based on one user and existing parking requirements.

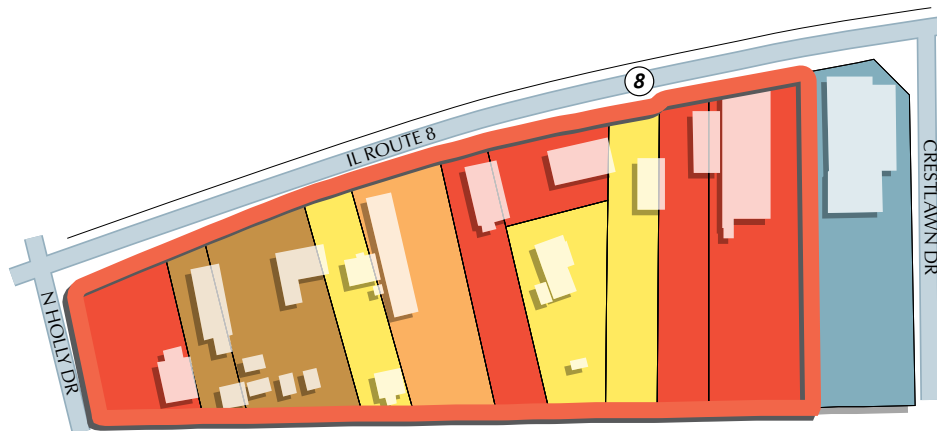
Existing Land Use

- Single Family Residential
- Multi-family Residential
- General Commercial
- Commercial Service & Office
- Public & Semi-Public

Context Map



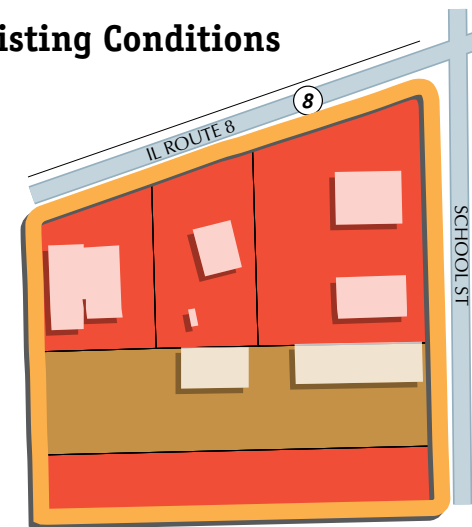
West End Existing Conditions



The illustrative development site in the West End currently consists of eleven parcels and a number of different land uses. Several parcels currently suffer from poor property maintenance and the land uses are generally incompatible with one another. Overall, they total approximately 4 acres in size, with over 700 feet of frontage along IL Route 8.

The North Tazewell Fire Department anchors the southwest corner of IL Route 8 and Crestlawn Drive and is envisioned to remain.

East End Existing Conditions

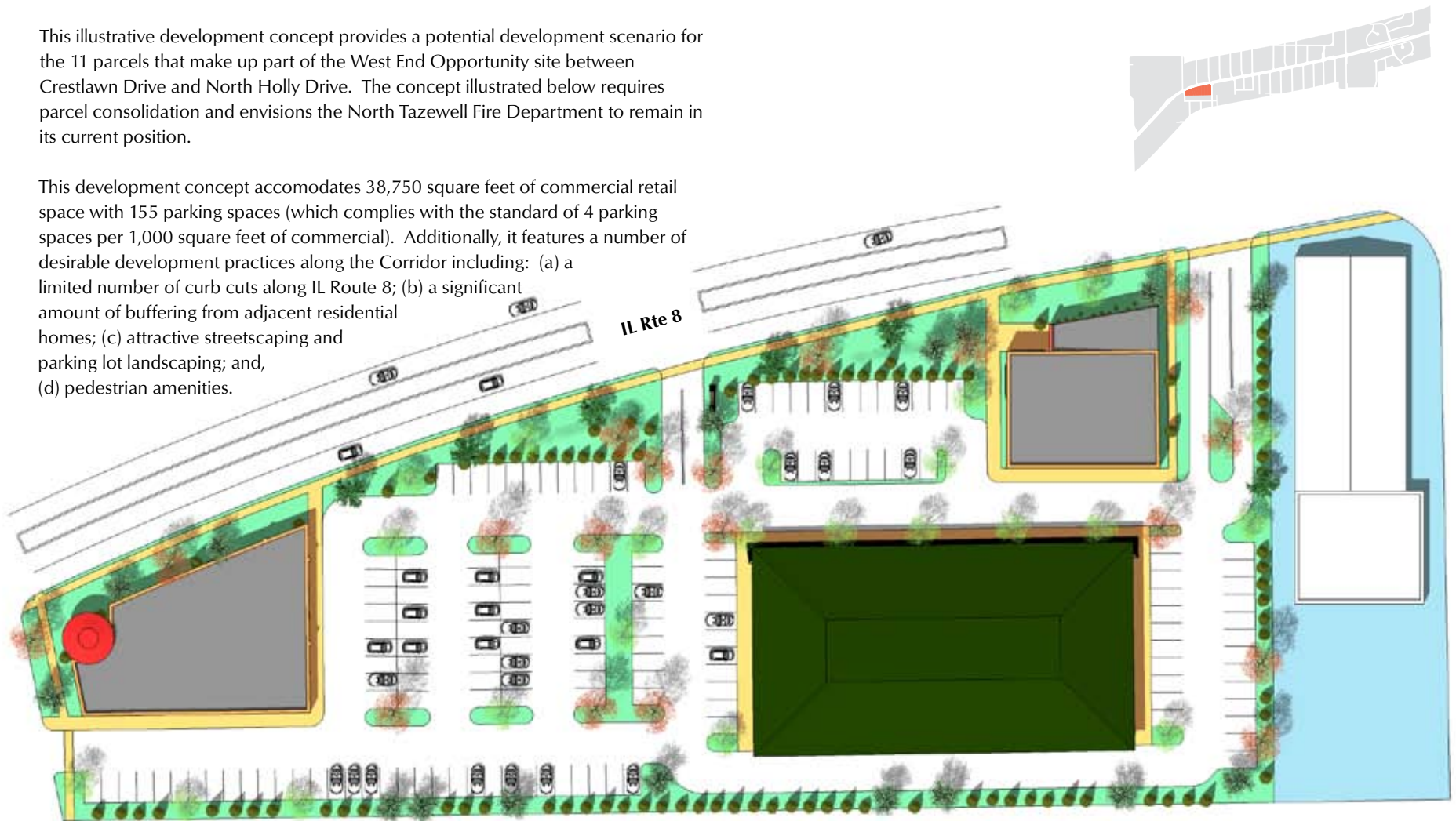


The East End is characterized mainly by existing commercial uses and one multi-family residential parcel. The development site consists of five parcels that appear to be underutilized in their current state. Overall, the East End development site totals approximately 3 acres in size, with over 400 feet of frontage along IL Route 8.

Figure Ten West End • Illustrative Redevelopment Concept A

This illustrative development concept provides a potential development scenario for the 11 parcels that make up part of the West End Opportunity site between Crestlawn Drive and North Holly Drive. The concept illustrated below requires parcel consolidation and envisions the North Tazewell Fire Department to remain in its current position.

This development concept accommodates 38,750 square feet of commercial retail space with 155 parking spaces (which complies with the standard of 4 parking spaces per 1,000 square feet of commercial). Additionally, it features a number of desirable development practices along the Corridor including: (a) a limited number of curb cuts along IL Route 8; (b) a significant amount of buffering from adjacent residential homes; (c) attractive streetscaping and parking lot landscaping; and, (d) pedestrian amenities.



Illustrative Development Concept • Plan View

Figure Eleven West End • Illustrative Rendering Concept A

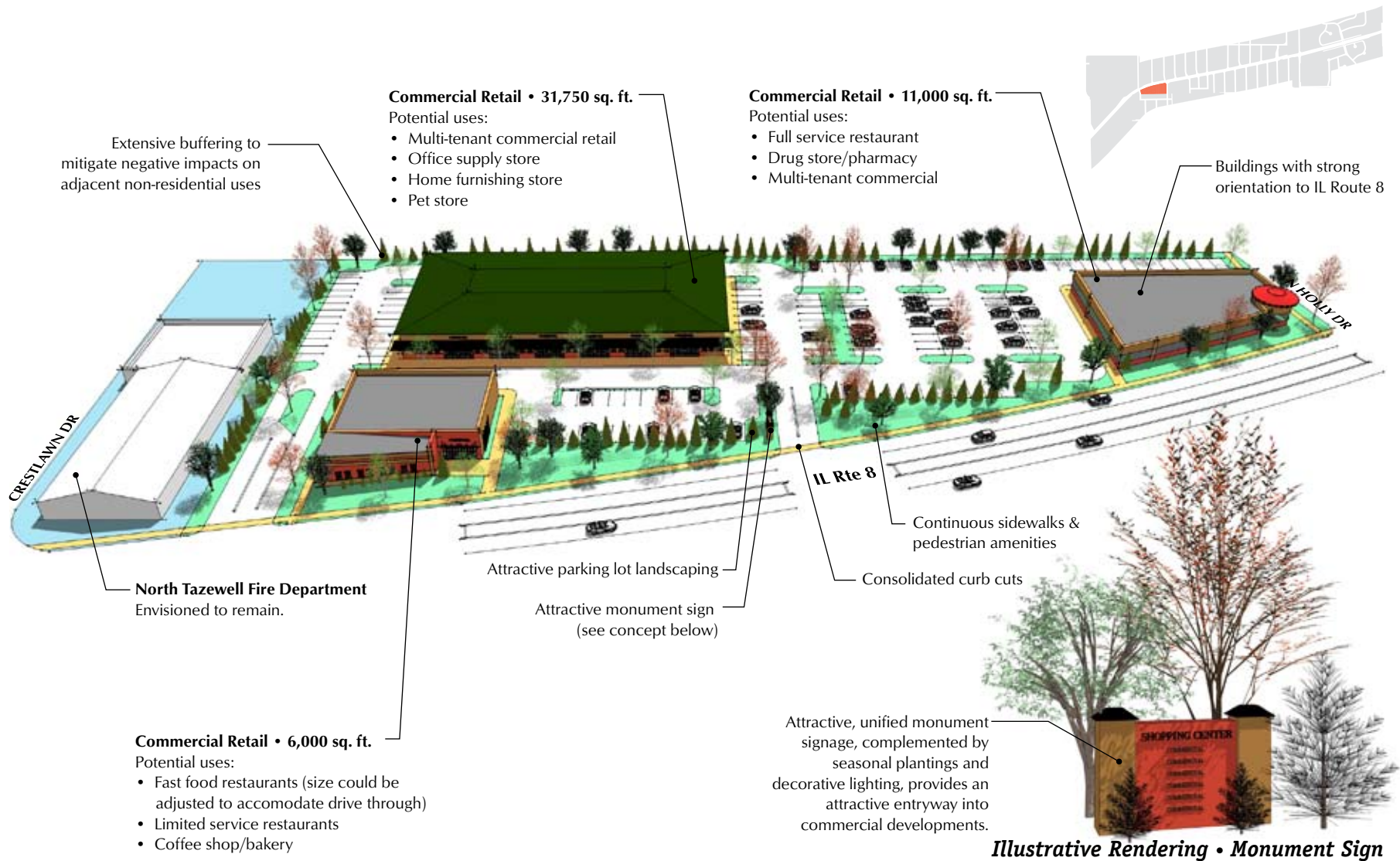


Figure Twelve West End • Illustrative Redevelopment Concept B

This illustrative development concept provides a potential development scenario for the 11 parcels that make up part of the West End Opportunity site between Crestlawn Drive and North Holly Drive. Similar to Concept A, this concept requires parcel consolidation to occur and envisions the North Tazewell Fire Department to remain in its current position.

This concept illustrates how one larger building can be accommodated on this site rather than a series of smaller commercial buildings. The building orientation and expansive parking lot fronting IL Route 8 differentiate this concept from Concept A; however, this concept does consolidate curb cuts and incorporate parking lot landscaping and buffering from adjacent non-commercial uses.

This development scenario can accommodate a building up to 48,000 square feet in size, with 190 parking spaces (consistent with the standard of 4 parking spaces per 1,000 square feet of commercial).



Illustrative Development Concept • Plan View



Figure Thirteen East End • Illustrative Redevelopment Concept A

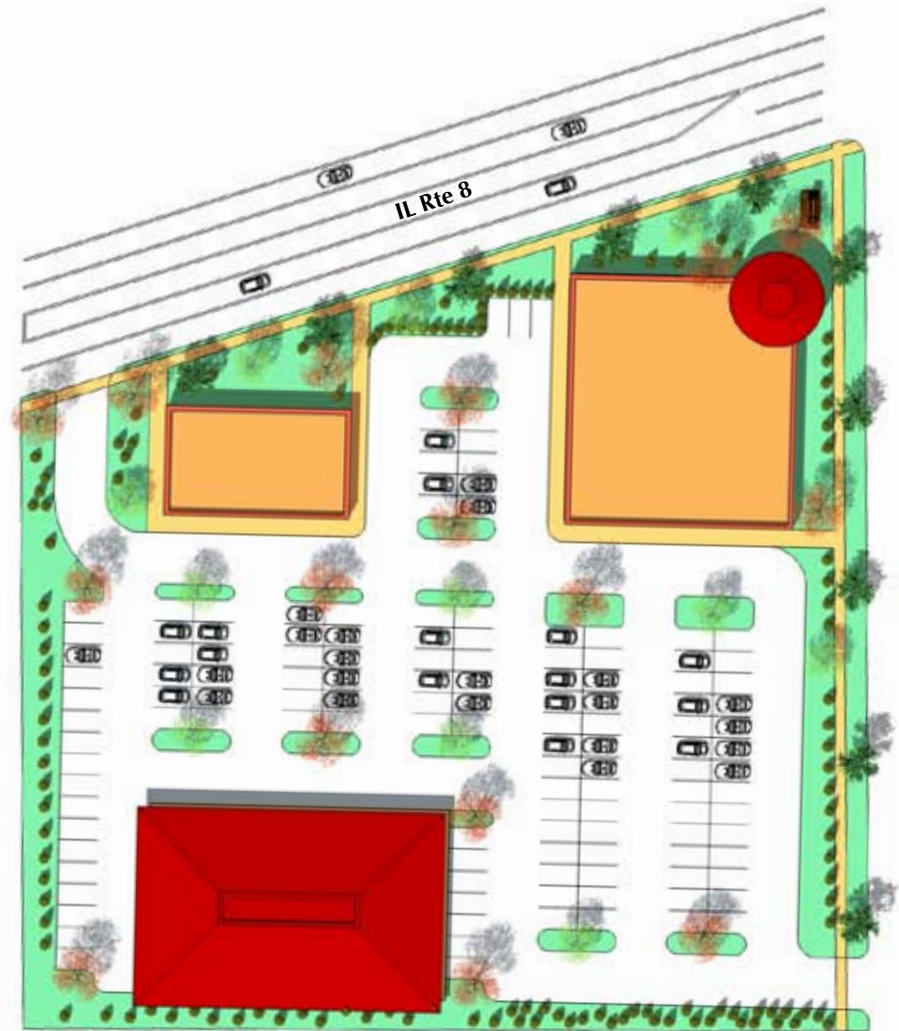


The East End development opportunity site is composed of 5 parcels on the south-west corner of IL Route 8 and School Street. The concept illustrated requires parcel consolidation to allow a unified development with shared parking to be constructed. This concept is provided for illustrative purposes only and should not be construed as a specific site recommendation.

Overall, this concept accommodates +/- 24,000 square feet of commercial space and 116 parking spaces (20 spaces more than required by the 4 per 1000 square feet of commercial space standard). This additional parking could be desirable for a restaurant as they typically require more parking than commercial retail uses.

This development concept features a number of desirable development practices along the IL Route 8 Corridor, including: (a) consolidation of curb cuts along IL Route 8; (b) a significant amount of buffering from adjacent residential homes; (c) attractive streetscaping and parking lot landscaping; and, (d) pedestrian amenities. Modifications to building footprints could be made to accommodate drive through facilities as appropriate.

The East End development site also functions as the eastern gateway to the IL Route 8 Corridor, thus should accommodate gateway signage and entryway features in highly visible locations at the intersection of IL Route 8 and School Street.



Illustrative Development Concept • Plan View

Figure Fourteen East End • Illustrative Rendering Concept A

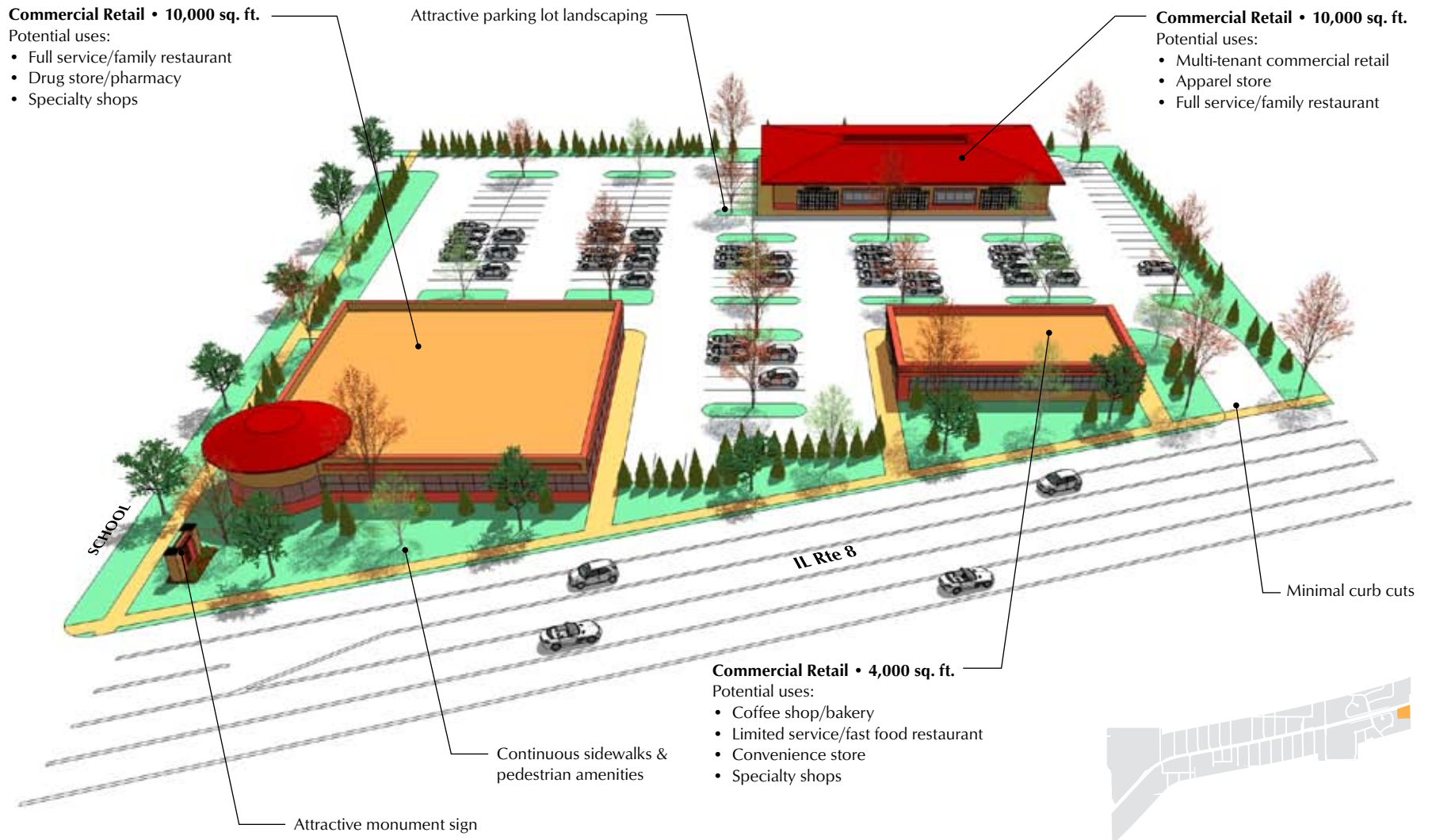
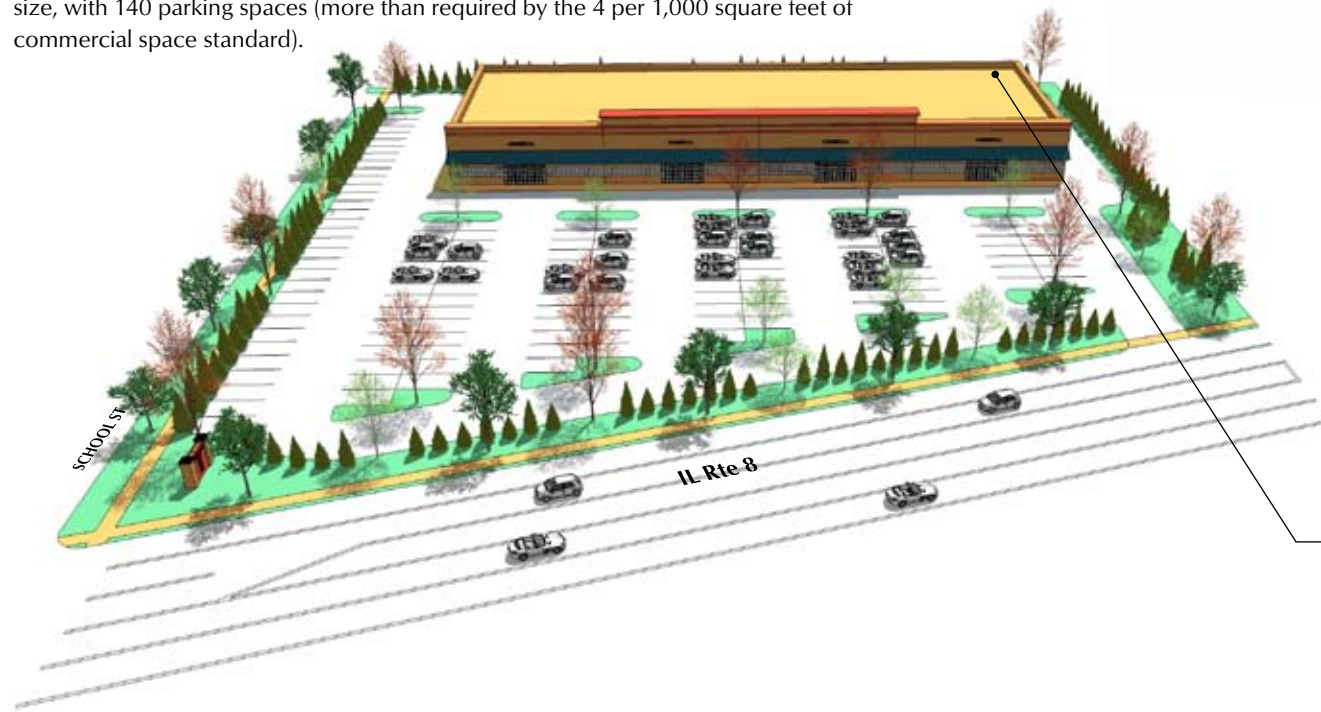


Figure Fifteen East End • Illustrative Redevelopment Concept B

This illustrative development concept provides a potential development scenario for the 5 parcels that make up the East End development opportunity site at the south-west corner of IL Route 8 and School Street. Unlike Concept A, this concept illustrates how one larger building can be accommodated on this site rather than a series of smaller commercial buildings. The building orientation and expansive parking lot fronting IL Route 8 help differentiate this concept from Concept A; however, this concept does incorporate consolidated curb cuts, parking lot landscaping and buffering from adjacent non-commercial uses. This concept is provided for illustrative purposes only and should not be construed as a specific site recommendation.

This development scenario can accommodate a building up to 30,000 square feet in size, with 140 parking spaces (more than required by the 4 per 1,000 square feet of commercial space standard).



**Illustrative Development Concept
Plan View**

Commercial Retail • 30,000 sq. ft.

Potential uses:

- Multi-tenant commercial retail
- Specialty grocer
- Arts & crafts store
- Bookstore
- Home furnishings

